

U.S. NAVAL BASE, PEARL HARBOR, GENERAL STOREHOUSE
(U.S. Naval Base, Pearl Harbor, Naval Shipyard, Facility No. 9)
Intersection of Avenue D & Seventh Street
Pearl Harbor
Honolulu County
Hawaii

HABS HI-487
HI-487

HABS
HI-487

PHOTOGRAPHS

WRITTEN HISTORICAL AND DESCRIPTIVE DATA

HISTORIC AMERICAN BUILDINGS SURVEY
PACIFIC GREAT BASIN SUPPORT OFFICE
National Park Service
U.S. Department of the Interior
1111 Jackson Street
Oakland, CA 94607

HISTORIC AMERICAN BUILDINGS SURVEY

U.S. NAVAL BASE, PEARL HARBOR, GENERAL STOREHOUSE
(U.S. Naval Base, Pearl Harbor, Naval Shipyard)
(Facility No. 9)

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Location: Intersection of Avenue D and Seventh Street
Pearl Harbor Naval Base
City and County of Honolulu, Hawaii

This building is at UTM coordinates 04.608280.2361460 and is within the boundaries of the Pearl Harbor, Naval Shipyard as defined in the location section of the overview report HI-483.

Significance: Facility 9 is the first storehouse built in the Navy Yard, as part of the base establishment of Pearl Harbor, under the Act of Congress of 13 May 1908. It is located within the Pearl Harbor National Historic Landmark.

Description: The building is a 3-story reinforced concrete structural frame, on concrete pier foundations, with hollow terracotta tile curtain walls. This was an uncommon method of construction at the time it was built. Concrete construction with its fire resistant characteristics was new technology at the time when the original structure was constructed. Most other storehouses were still constructed of wood. A basement is located under a portion (end 8 bays of the addition side) of the building. The original dimensions of the building were, length 252' - 6" (15 bays at 16'-9" center to center), width 61' - 4" (3 bays at 20'-0" center to center), and height from grade to eaves of 35' - 0". The building footprint was almost doubled in 1918 when 12 more bays (16'-9" center to center) having a total length of 201'-0" were added onto the length of the building (This addition matches the original building's style such that the building appears to have been built as one singular structure.). The storehouse provides offices, toilets and storerooms in each story with a vault in the second story. An elevated loading dock is located on the north side of the building. Under the building foundation is a coral and rock fill.

The building has parapets at the northwest and southeast ends and the center of the other two (long) elevations. Between them the low-slope roof eaves protrude a short distance. The short eaves are supported by bracketed string course spaced four to a bay, giving the building a hint of Classical Order. Copings at the parapet walls are of precast concrete. The two elevator shafts projecting through the roof are concrete structures each of slightly different designs. The original side structure has a flat roof with projecting eaves and 4-light steel pivot windows and wood louvered windows on all four elevations. The elevator shaft room on the addition side has a slightly classical look with a molded cornice, modillions and parapet wall projecting above

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these edifices. The roof covering at the original side is of tar and gravel. The roof covering at the addition side was rolled asphalt roofing.

All pilasters, columns, girders, beams, sills, exterior stairs in elevator well, second floor, third floor, and roof are of reinforced concrete construction. The columns are organized on a grid layout. They measure 1'-10" square. On the first floor, they are encased in ½" thick steel at bases up to the 4'-0" height. The columns on the second and third floor are protected by steel corners up to the 4'-0" height. All floors are of reinforced concrete. The floor to ceiling height of the first and second floors is 12'-0" and the third floor is 11'-3".

All exterior walls between the pilasters and girders and all interior partitions, including the stair and elevator enclosure, were originally constructed of hollow terra-cotta tile finished on both sides with 1" thick cement plaster. All exterior concrete work, including the exterior plaster finish, is dampproofed with a dampproofing compound applied by the integral method. The floor beams are formed having a 45-degree beveled edge where the beam and floor intersect. All interior girders, beams and ceilings are finished with a coat of cement.

The main stair is located in the central area of the present building, at the end (15th) bay of the original portion. It is recorded as having been built as a cast iron stair. However, it is presently a poured-in-place concrete stair with pipe railing leads from the first-story to the third-story. The stair hall is 5'-9" wide with a coffered ceiling. The door into the stair hall, though no longer there, was a wood frame single swing wood panel door.

The building is equipped with two electric freight elevators, one in the original section and one in the addition. The electric freight elevator shaft for the original portion is placed in the center front of the building with the outside wall omitted leaving the elevator well open. The elevator is of steel frame construction, has a capacity of 5,000 pounds and a speed of 50 feet-per-minute. The car rise is from the first to the third floor, a distance of approximately 24'. In the elevator shafts between the elevator and the inside wall of the shaft there is a 3'-0" wide reinforced concrete stair running from the first floor to the roof. A balustrade of pipe stanchions and railings is used on the stairs and platforms. The stair in the elevator well on the original side appears to have been removed. In 1934, the elevators were improved. The original storehouse portion was outfitted with a 220 volt, 3 phase, 60-cycle elevator having a capacity of 5000 pounds and a speed of 50 F.P.M. The extension portion was outfitted with a 220 volt, 3 phase, 60-cycle elevator that had a capacity of 10,000 pounds and a speed of 75 F.P.M. The elevators have wood floors and have kept their original mechanics and buttons.

Few of the exterior doors and fenestration is original. The sliding exterior doors have been removed and infilled with CMU block. They

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were in two leaves, hung on Coburn trolleys and were provided with roller guides, heavy iron hasps and brass padlocks each with outside door openings that had iron wheel guards. The sliding entry door has been replaced with an aluminum sash double door with overhang. Fenestra steel sash were originally used for all the windows and glazed with ribbed wire glass (except in the offices where plain glass was installed). These were multi-light steel windows with a central pivot window section for ventilation. However, very few still exist. All of the first floor windows have been removed and infilled with CMU block while many of the second floor windows have been removed and infilled with CMU block or glass jalousies. All of the third floor windows have been replaced with glass jalousie windows.

Little of the interior fixtures and surfaces are original. The original incandescent bulb lighting with simple fixtures placed near the ceiling is no longer in existence. The ceilings are drop ceilings with acoustical panels and flush fluorescent lighting. Vinyl tile covers the floors. The walls on one side of the building are covered with acoustical paneling so that no windows can be seen from the interior. Gypsum board walls have been added in areas to divide interior spaces.

Gutters, downspouts and flashing are of 16-ounce copper. All metal work except copper was originally painted with two coats of white lead and oil. Hardware is of steel. The original plumbing included the usual porcelain toilet fixtures installed in accordance with the Standard Plumbing Specification issued by the Treasury, War and Navy Department, which have all been replaced with modern fixtures. Fire risers are installed for fire protection, two on the front and two on the rear walls. (Robert Walden, 1945).

Historical Context:

The Supply Division of the Industrial Department was responsible for the main supply functions. In the early decades of the Pearl Harbor base, most storehouses were under the Supply Division. This 1913 building was the only general storehouse of permanent construction on the installation until 1918.

This building was built under the Act of Congress of 13 May 1908 that authorized the first major expenditure of funds to develop Pearl Harbor. The Act of Congress directed the Secretary of the Navy to erect at Pearl Harbor "all the necessary machine shops, storehouses, coal sheds, and other necessary buildings, and to build there one graving dry dock capable of receiving the largest war vessels of the Navy." By mid-1913, many major buildings in the Navy Yard were completed or under contract (Yoklavich 2000: 12, 13). Facility 9 was built as part of this early group of buildings at Pearl Harbor including the shipfitters and boiler shop (Fac. 4 HABS HI-359), the pipe and plumbing shop (Fac. 2, now gone), and the woodworking shop (Fac. 7, HABS HI-486), the powerhouse (Fac. 8, HABS HI-446), the administration building (Fac. 1, HABS HI-407), a repair shop (Fac. 5,

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HABS HI-484), a steel storehouse (Fac. 13, HABS HI-462), a refrigeration house (Fac. 18) and six sets of Navy Yard quarters.

By 1917, the building size was determined too small to handle the amount of storage necessary during this time of Naval Shipyard expansion. Thus, the building was enlarged by 12 bays in 1918 for a total of 25 bays.

The building, though designed as a storehouse, has gone through several uses over the years. Although it was built to function as a storage facility, prior to World War II part of the building was used as office space for the Shipping Division of the Navy before the office was moved to Facility 167. In a report found in the Walden files entitled "Yard Supply", it states, "On December 7, 1941, the office of this Division [Shipping] was located in Facility 9, Navy Yard. In November of 1942, the office was moved to new and improved quarters in Facility 167. The space occupied by the office and baggage room has been increased three times since November 1942. This increase was necessitated by the increase in "clerical personnel and the baggage processed" (Walden, c. 1945).

In 1942, Facility 9 was turned over to the Industrial Department. In a letter from the Commandant of the Navy Yard at Pearl Harbor to the Commandant of the Fourteenth Naval District dated October 17, 1942, Facility 9 was urgently required to accommodate industrial activities once it was evacuated by the Supply Department (Commandant, Navy Yard, Pearl Harbor, T.H., 1942). The building was used as a general storehouse and a radio materials workshop. The radio materials workshop was located on the third floor, and the entire third floor was redesigned to hold a radar office, radar workshop, receiver repair rooms, and a vault.

In 1944, a Radar School Extension, which was a wooden structure that sat on the roof of Facility 9, was designed and built and included a testing platform. The drawings are entitled Industrial Shop Facility 9, indicating that the building name was modified between 1942 and 1944.

By the end of the War, the function of this facility was converted entirely from a storage facility to a radio and electronics repair facility. Based on drawings of a new CRF vault constructed at the southeast corner of the building of 1945, the building was renamed the Radio Material Office Building after 1944. Again in 1959, as-built basement-foundation and first floor plans show major renovations to the first floor spaces. A transmitter repair area, cryptographic repair facility, and teletype repair areas were designed to handle its new functions. The drawings are entitled Electronics (Shop 67), indicating that the building name was modified sometime in the 1950s. Also by this time, the adjacent building to the northwest (Facility 9A – not covered in this report) was built as a radar antenna shop.

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With the shift from using mainly electric energy sources to nuclear energy sources during the Cold War period, the building use has had to adjust as necessary. In 1968, the building was reorganized and renovated, and used as a Nuclear Overhaul Facility. "Clean Rooms" were added, interior walls were built, and pipe fit-up and welding rooms were designed. A new mechanical and electrical system was installed.

The current name of the building is a Nuclear Repair Shop. It is used as the NPPSO (Navy Publishing and Printing Service Office) Copier Room for the Pearl Harbor Shipyard.

For an overview of the Naval Shipyard see HABS No. HI-483.

Sources:

The original drawings for this building are on microfilm or electronic scan at NAVFACPAC Plan Files. Some later drawings for Facility 9 are also listed in the NAVFACPAC microfilm index.

Commandant, Navy Yard, Pearl Harbor, T.H.

1942 Memo to the Commandant, Fourteenth Naval District, dated 17 Oct. 1942. In files of National Archives and Records Administration, San Bruno, RG 181, 14th ND District Staff Hdqtrs, General Correspondence [Formerly Classified] 1936-1944, Box 2, Folder A1-1/p 11/ND 14 Development-Schools.

National Archives II, Still Photo Collection

Photos in group RG71CA, Box 165, "General Storehouse" folder.

Robert Walden Collection

[1945] "Yard Supply", typescript report, filed at UH Hamilton Library War Records Department.

U.S. Navy, Bureau of Yards and Docks

[1913] Illustrated Reports of Construction Contracts, General Storehouse (Fac. 9) at National Archives II, Still Photo Section in RG 71CR, Box 2, Vol. 3.

Yoklavich, Ann

2000 U.S. Naval Base, Pearl Harbor, Overview Report. Historic American Building Survey documentation for facilities at the Shipyard of the U.S. Naval Base, Pearl Harbor. Submitted to and accepted by the National Park Service as HABS No. HI- 483. Prepared by Mason Architect and Franzen Photography, for the U.S. Navy, Pacific Division Naval Facilities Engineering Command.

Project information:

Photo documentation and recordation of this facility by the Navy has been done in anticipation of future alterations or potential demolition of the structure. Photo documentation of historic facilities by the Navy

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assists in expediting planned undertakings by having the documentation prepared prior to taking actions. Also, photo documentation assists the Navy in gaining more information about its historic facilities to assist in making proactive management decisions. This project is being supervised by Jeffery Dodge A.I.A., Historical Architect NAVFAC Hawaii. The photographic documentation was undertaken by David Franzen, photographer. Lorraine M. Palumbo, Ph.D. Architectural Historian, of Mason Architects, Inc. prepared the written documentation. The field work and research was conducted for this report between July 2001 and December 2001.

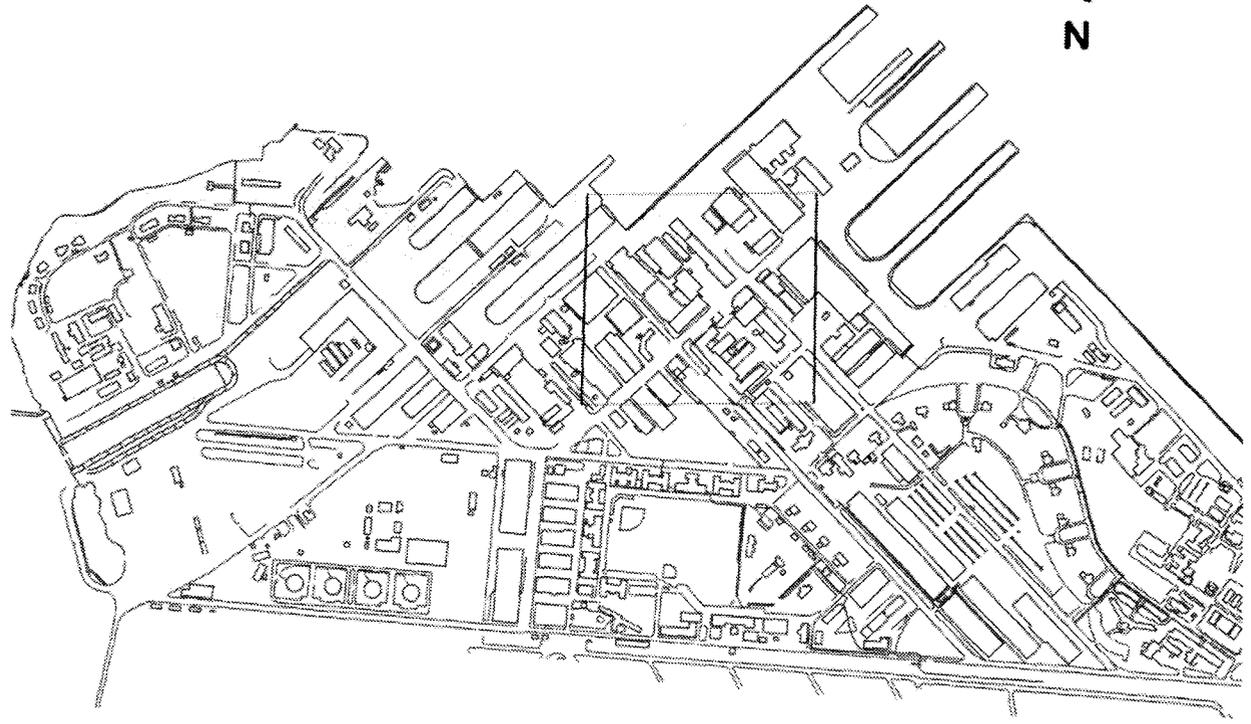
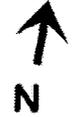
Prepared by:

Lorraine M. Palumbo, Ph.D., Architectural Historian
Mason Architects, Inc.
119 Merchant Street, Suite 501
Honolulu, HI 96813
January 2003

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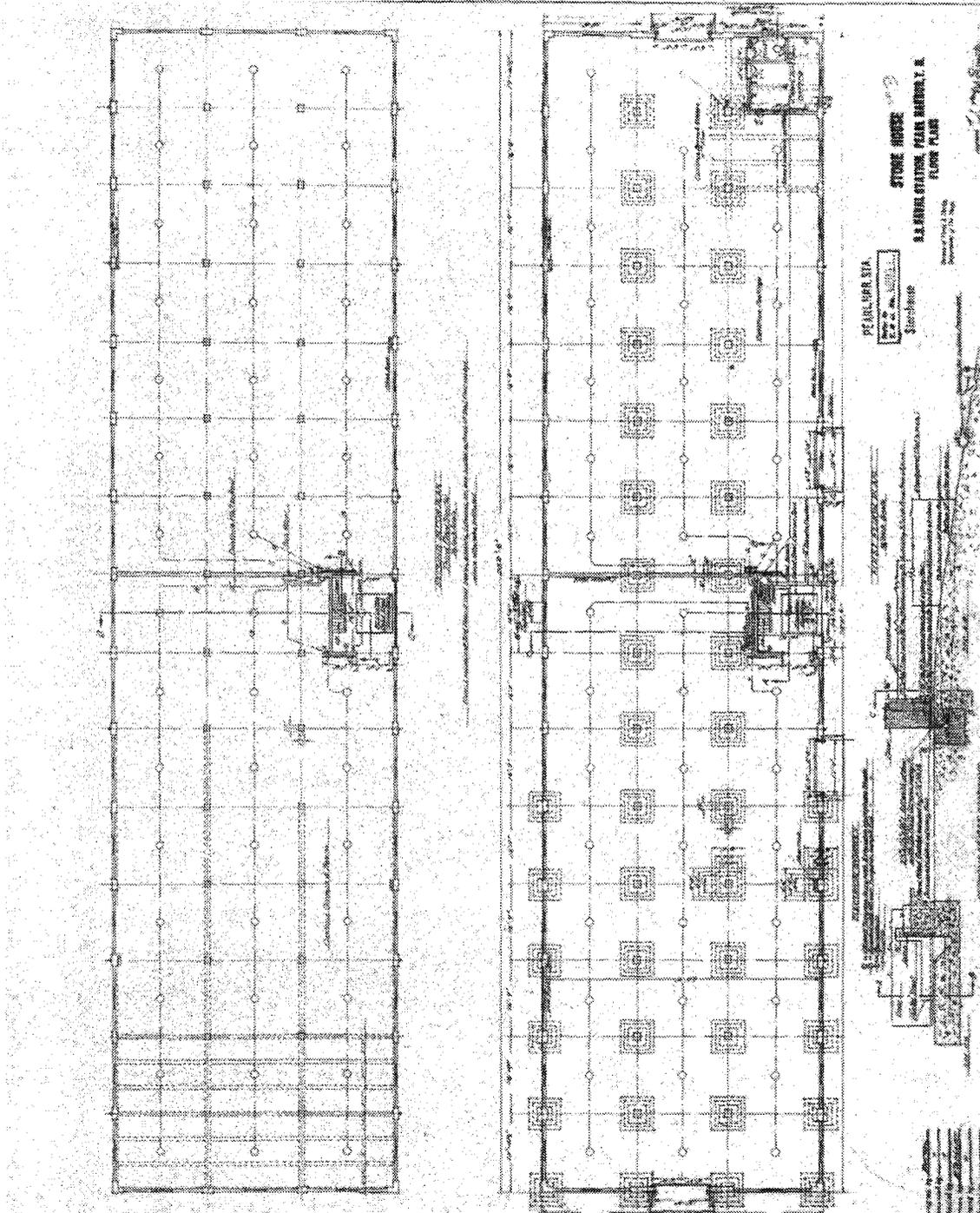
Shipyard Map

SEE ENLARGED MAP



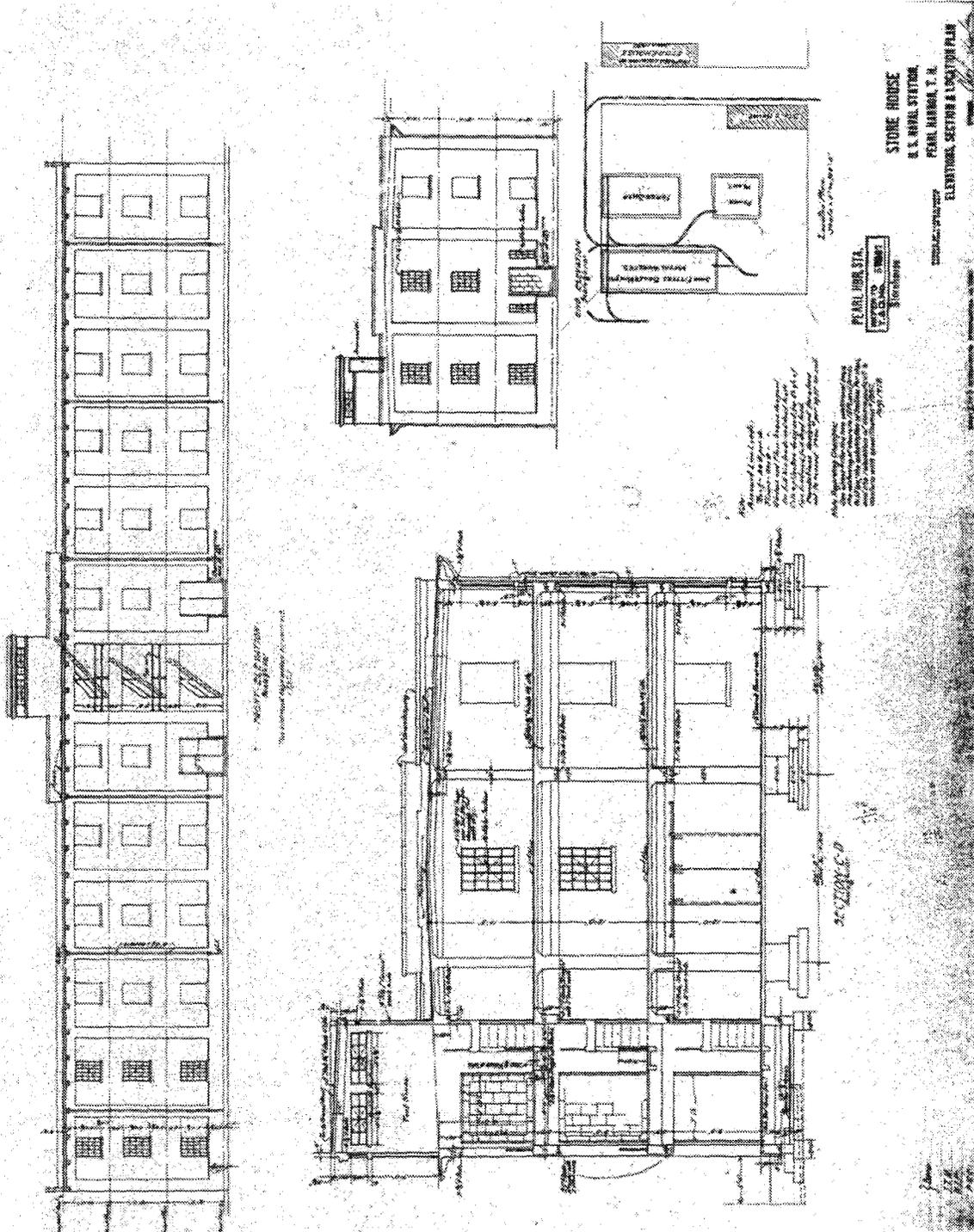
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Original Construction Drawing, First and Second Floor Plans
(Drawing No. 60053, dated 8/13/1912) (reduced)



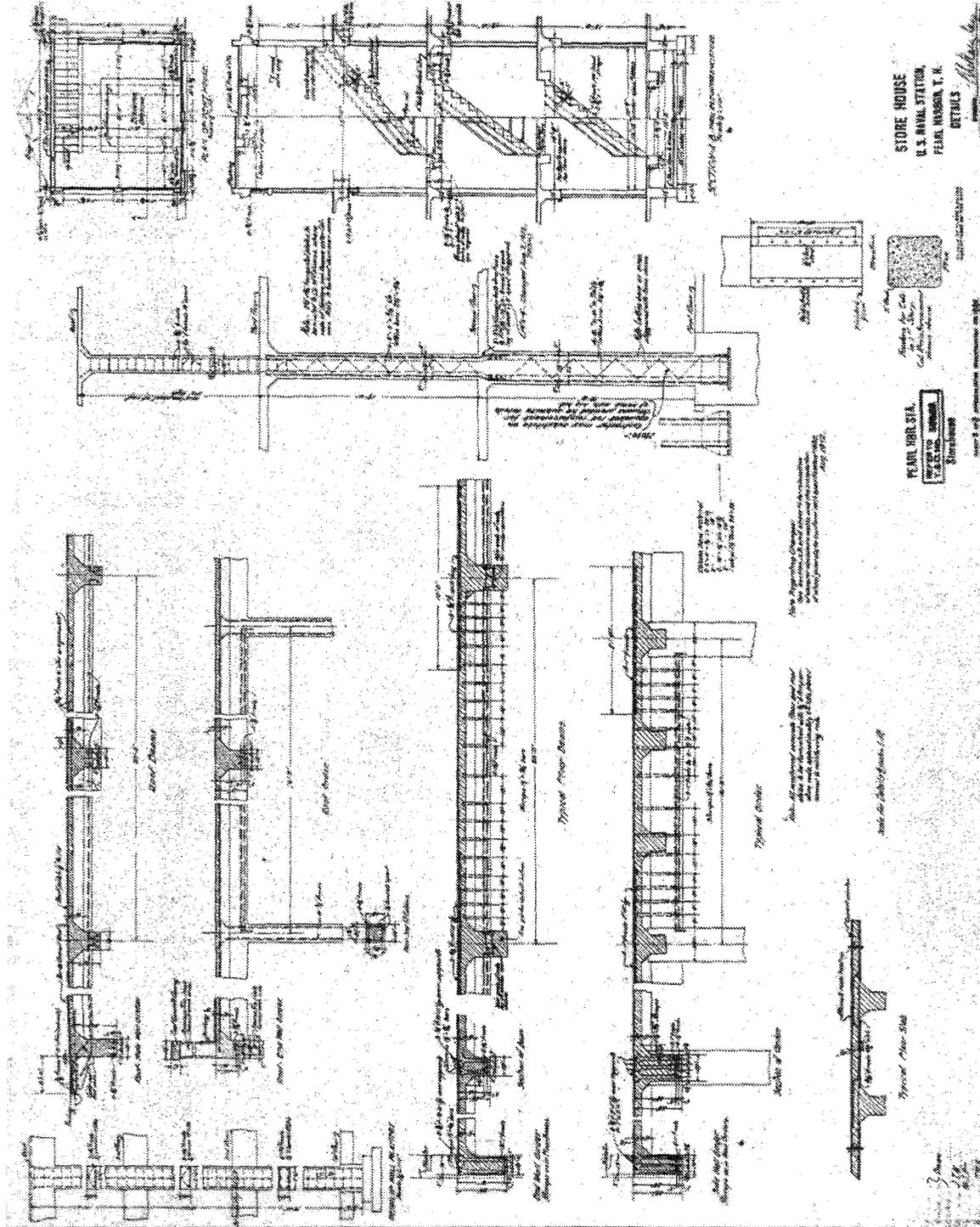
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Original Construction Drawing, Building Elevations, Section, and Location Plan
(Drawing No. 57937, dated 7/10/1911) (reduced)



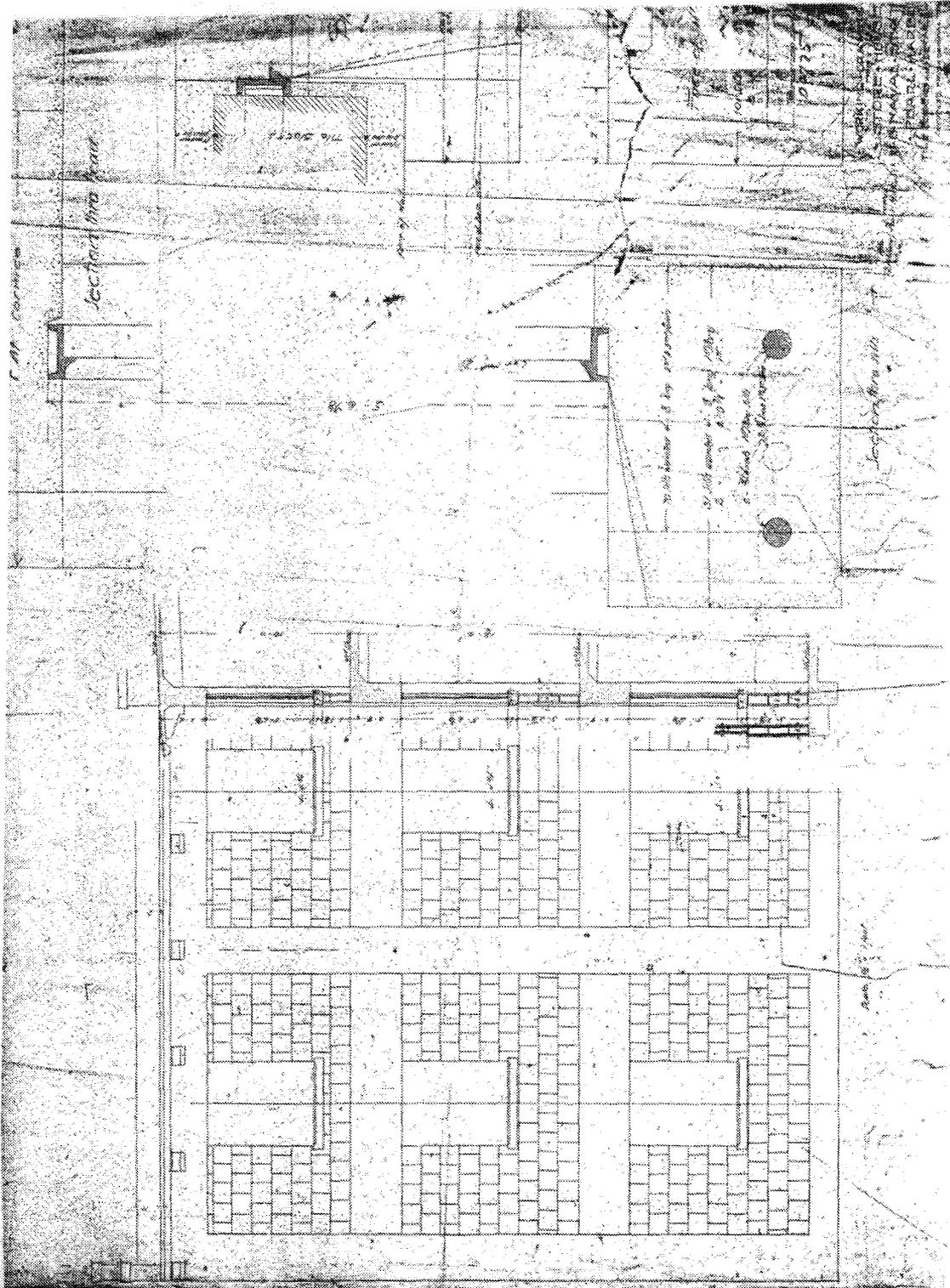
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Original Construction Drawing (partial sheet), Details
(Drawing No. 57938, dated 7/10/1911) (reduced)



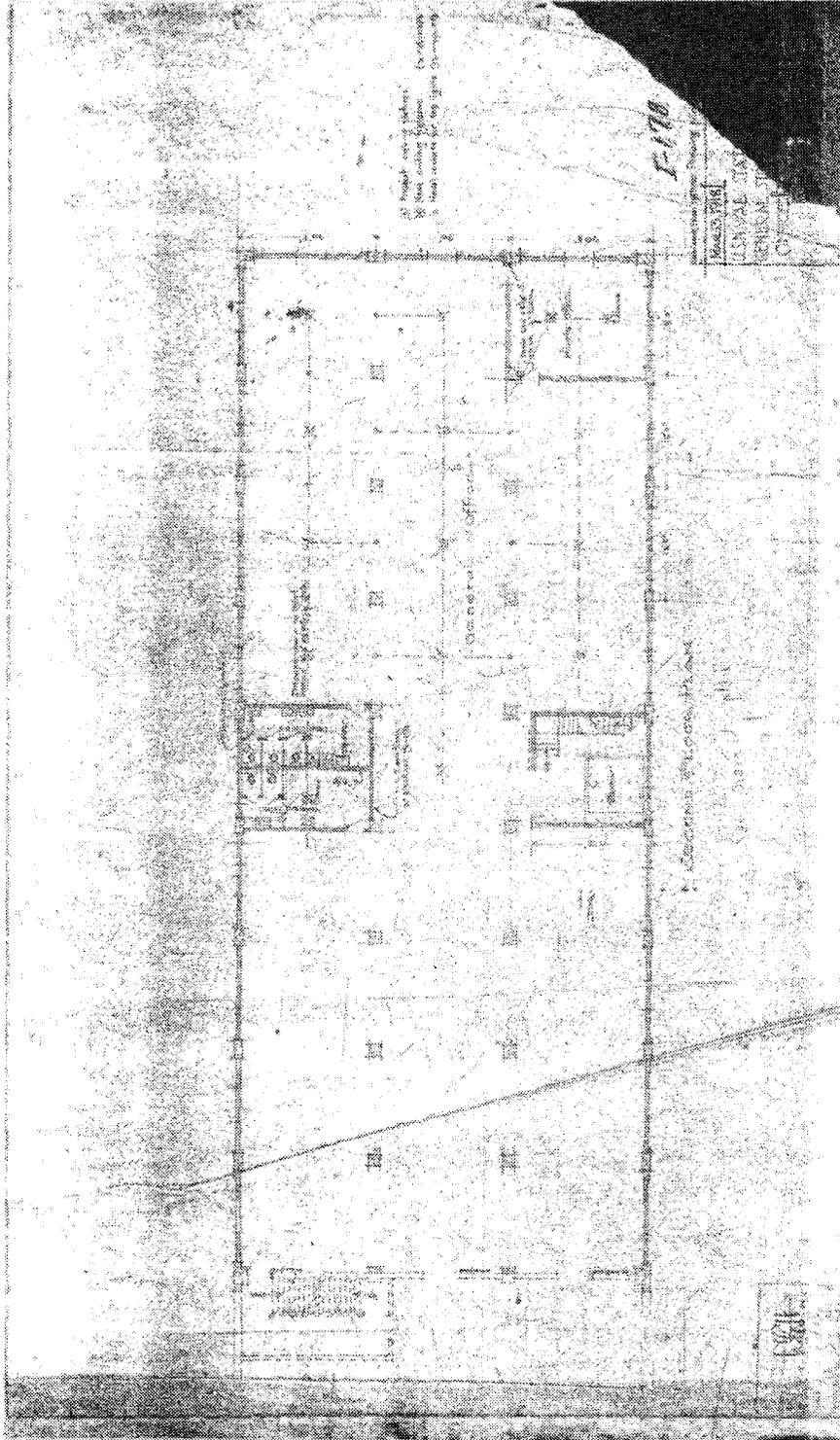
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**Original Construction Drawing (partial sheet), Tile Blocks and Details of Elevation
(Drawing No. 728, dated 2/13/1912) (reduced)**



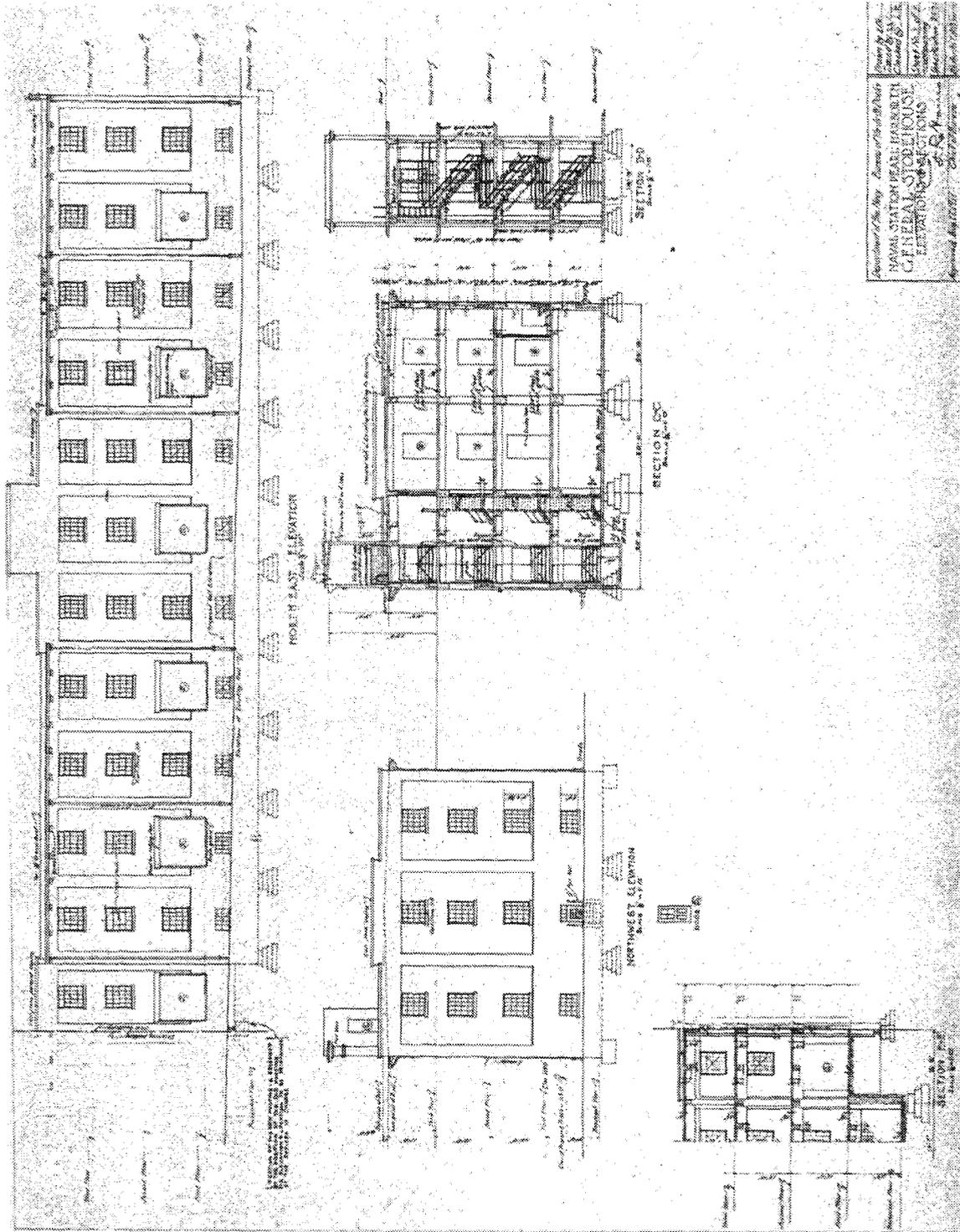
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**1918 Building Addition Construction Drawing, Floor Plan,
(Drawing No. I-178, dated 3/13/1918) (reduced)**



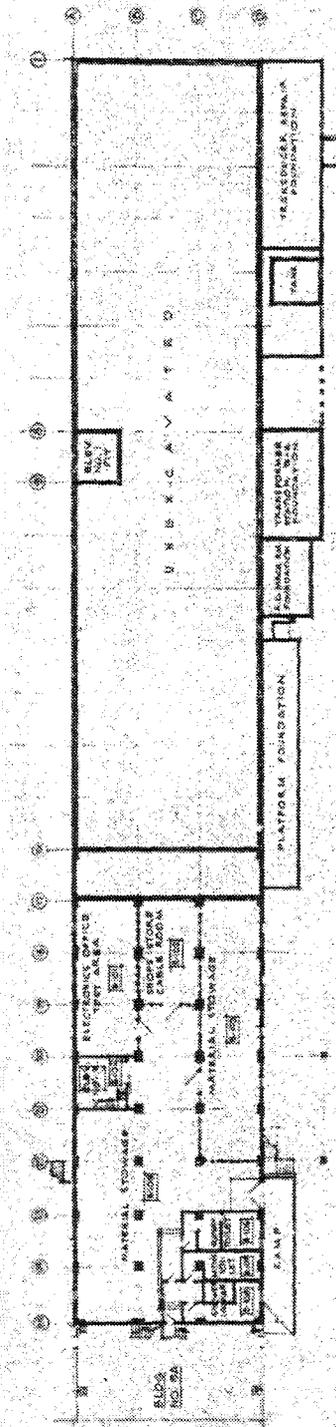
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1918 Building Addition Construction Drawing, Elevations and Sections
(Drawing No. 70757, dated 8/27/1917) (reduced)

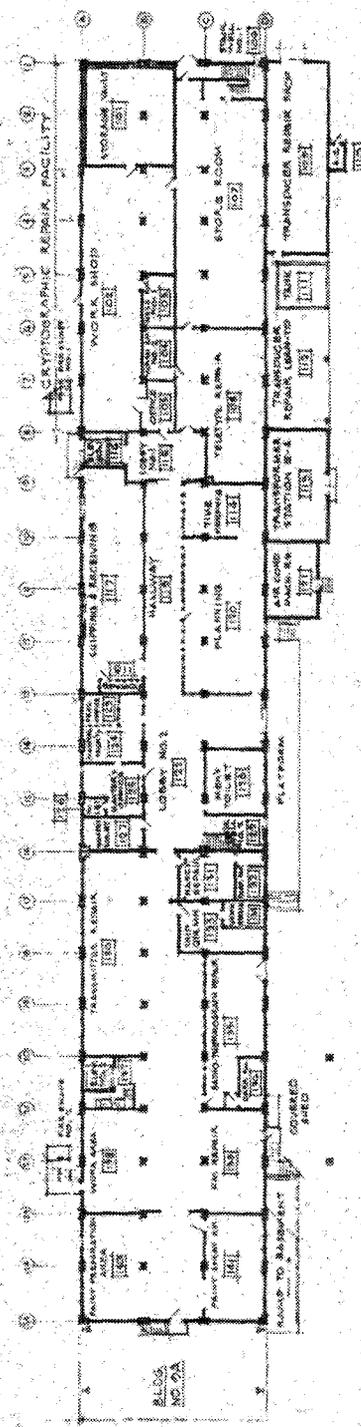


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1959 Renovation Construction Drawing, As-built Basement-foundation and First Floor Plans (Drawing No. 852680, dated 10/13/1959) (reduced)



BASEMENT AND FOUNDATION PLAN



FIRST FLOOR PLAN

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Historic photo of Facility 9 taken on March 1, 1913.
From National Archives RG71 CA

